



Cultural Heritage Evaluation Report, Old Fort Road Overhead Bridge, Simcoe County Structure # 058086, County Road 58, Tay Township, Simcoe County, Ontario

Project number: PHC-2019-1054

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Simcoe County

Proponents: : LEA Consulting Ltd.

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Executive Summary

Parslow Heritage Consultancy Inc. (PHC) completed a Cultural Heritage Evaluation Report on behalf of the County of Simcoe, as part of a Schedule B Municipal Class Environmental. The County of Simcoe is proposing to replace the Old Fort Overhead Bridge (Simcoe Structure #058086) located on Old Fort Road (Simcoe County Road 58) over the abandoned CN Railway that has been converted into the Trans-Canada Trail. The existing Old Fort Overhead Bridge consists of 3 spans (13.6m – 13.7m - 12.1m) of concrete slab on prestressed voided slab and reinforced concrete piers and abutments. Three alternatives for the replacement of the Old Fort Road bridge are considered as part of the Schedule B Municipal Class Environmental Assessment:

- ▶ Replacement of the bridge with one of similar style and construction;
- ▶ Replacement of the bridge with a tunnel structure with earth embankment; or
- ▶ Replacement of the bridge with at-grade intersection of the road and trail.

The purpose of this assessment is to review relevant historical documents and evaluate the potential cultural heritage value or interest of the existing structure. To evaluate potential cultural heritage value or interest the standards of the *Ontario Heritage Act* (OHA) under Regulation 9/06 were applied. Additionally, the *Ontario Heritage Bridge Guidelines for Provincially Owned Bridges* (MTO 2008) were consulted.

A site visit was conducted on May 21, 2020, to document the structure and surrounding landscape. Following the site visit, review of available historic records, the application of Ontario Regulation 9/06, as well as the criteria set forth in the *Ontario Heritage Bridge Guidelines for Provincially Owned Bridges*, it has been determined that the current structure exhibits no cultural heritage value or interest and as such does not warrant a Heritage Impact Assessment (HIA). As a result of the CHER the following recommendation is made:

1. The Old Fort Road Overhead Bridge (Simcoe Structure #058086) does not exhibit cultural heritage value or interest and as such no further cultural heritage assessments are recommended for this structure.

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1. Introduction

Parslow Heritage Consultancy Inc. (PHC) completed a Cultural Heritage Evaluation Report on behalf of the County of Simcoe, as part of a Schedule B Municipal Class Environmental. The County of Simcoe is proposing to replace the Old Fort Overhead Bridge (Simcoe Structure #058086) located on Old Fort Road (Simcoe County Road 58) over the abandoned CN Railway that has been converted into the Trans-Canada Trail. Three alternatives for the replacement of the Old Fort Road bridge are considered as part of the Schedule B Municipal Class Environmental Assessment (See Appendix A):

- ▶ Replacement of the bridge with one of similar style and construction;
- ▶ Replacement of the bridge with a tunnel structure with earth embankment; or
- ▶ Replacement of the bridge with at-grade intersection of the road and trail.

The purpose of this assessment was to review relevant historical documents and evaluate the potential cultural heritage value or interest of the existing structure. To evaluate potential cultural heritage value or interest the standards of the *Ontario Heritage Act* (OHA) under Regulation 9/06 were applied.

A site visit was conducted on May 21, 2020.

1.1 Legislative and Policy Framework

The Ontario Heritage Act is the primary piece of legislation that determined policies, priorities and programs for the conservation of Ontario's heritage. There are many other provincial acts, regulations and policies governing land use planning and resource development that support heritage conservation, including:

- ▶ the Planning Act, which states that "conservation of features of significant architectural, cultural, historical, archaeological or scientific interest" (cultural heritage resources) is a "matter of provincial interest". The Provincial Policy Statement, 2020, issued under the Planning Act, links heritage conservation to long-term economic prosperity and requires municipalities and the Crown to conserve significant cultural heritage resources.
- ▶ the Environmental Assessment Act, which defines "environment" to include cultural conditions that influence the life of humans or a community. Cultural heritage resources and cultural heritage landscapes are important components of those cultural conditions.

Environmental Assessment Act

This CHER is being completed pursuant to the *Environmental Assessment Act*. As such, this report addresses above ground cultural heritage resources over 40 years old, Simcoe County Structure # 058086. Use of a 40-year-old threshold is a guiding principle when conducting a preliminary identification of cultural heritage resources (Ministry of Transportation 2006; Ministry of Transportation 2007; Ontario Realty Corporation 2007).

Under the Environmental Assessment Act (1990) environment is defined in Subsection 1(c) to include:

- ▶ Cultural conditions that influence the life of man or a community, and;

- ▶ Any building, structure, machine, or other device or thing made by man.

Ontario Heritage Act

The criteria for determining cultural heritage value or interest are outlined in the *Ontario Heritage Act* (OHA) under Regulation 9/06. (1) The criteria set out in subsection (2) are prescribed for the purposes of clause 29 (1) (a) of the Act. (2) A property may be designated under section 29 of the Act if it meets one or more of the following criteria for determining whether it is of cultural heritage value or interest:

1. The property has design value or physical value because it,
 - i) Is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii) Displays a high degree of craftsmanship or artistic merit, or
 - iii) Demonstrates a high degree of technical or scientific achievement.
2. The property has historical value or associative value because it,
 - i) Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii) Yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii) Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
3. The property has contextual value because it,
 - i) Is important in defining, maintaining or supporting the character of an area,
 - ii) Is physically, functionally, visually or historically linked to its surroundings, or
 - iii) Is a landmark.

Ontario Heritage Bridge Guidelines for Provincially Owned Bridges

The *Ontario Heritage Bridge Guidelines for Provincially Owned Bridges* (OHBG) were developed in 1993 by the Ontario Ministry of Transportation (MTO); the current version of the document was revised in 2008 (MTO 2008). While Old Fort Road Overhead Bridge is not provincially owned, the established assessment guidelines provide a methodology by which to assess the potential cultural heritage value of interest of the structure in question. This is accomplished by way of an evaluative scoring system derived from the criteria outlined in Ontario Regulation 9/06 and calibrated by the MTO (MTO 2008). The scoring system requires an overall score of 60 to be achieved before a bridge can be considered to exhibit cultural heritage value or interest. Appendix A provides a copy of the scoring system employed.

Planning Act

The *Planning Act (1990)* provides the legislative framework for land use planning in Ontario. Part 1, Section 2 (d) and (r) of the Act identifies matters of provincial interest.

Part I, Section 2

The Minister, the council of a municipality, a local board, a planning board and the Tribunal, in carrying out their responsibilities under this Act, shall have regard to, among other matters, matters of provincial interest such as,

(d) the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest;

(e) the promotion of built form that,

(i) is well-designed,

(ii) encourages a sense of place, and

(iii) provides for public spaces that are of high quality, safe, accessible, attractive and vibrant.

Provincial Policy Statement (2020)

The Provincial Policy Statement (PPS), issued under Section 3 of the *Planning Act*, came into effect on May 1, 2020. It applies to all planning decisions made on or after that date and replaced the PPS, 2014. The PPS provides direction for the appropriate regulation for land use and development while protecting resources of provincial interest, and the quality of the natural and built environment, which includes cultural heritage and archaeological resources. These policies are specifically addressed in Part V, Sections 1.7 and 2.6.

Section 1.7.1e of the PPS addresses long-term economic prosperity by “encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes”.

Section 2.6 of the PPS addresses the protection and conservation cultural heritage and archaeological resources in land use planning and development and requires the following:

2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

2.6.2 Development and site alteration shall not be permitted on lands containing archaeological resources or areas of archaeological potential unless significant archaeological resources have been conserved.

2.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.

2.6.4 Planning authorities should consider and promote archaeological management plans and cultural plans in conserving cultural heritage and archaeological resources.

2.6.5 Planning authorities shall engage with Indigenous communities and consider their interests when identifying, protecting and managing cultural heritage and archaeological resources.

1.2 Regional and Municipal Policy

1.2.1 County of Simcoe Official Plan

The Official Plan of the County of Simcoe (the Plan) is prepared under the *Planning Act* R.S.O 1990 c.P. 13, as amended, (Planning Act) of the Province of Ontario. The Plan provides a policy context for land use planning taking into consideration the economic, social, and environmental impacts of land use and development decisions.

The goals of this Plan are:

- ▶ To protect, conserve, and enhance the County's natural and cultural heritage;
- ▶ To achieve wise management and use of the County's resources;
- ▶ To implement growth management to achieve lifestyle quality and efficient and cost effective municipal servicing, development and land use;
- ▶ To achieve coordinated land use planning among the County's local municipalities and with neighbouring counties, districts, regions, and separated cities, and First Nations lands;
- ▶ To further community economic development which promotes economic sustainability in Simcoe County communities, providing employment and business opportunities; and
- ▶ To promote, protect and enhance public health and safety.

Section 4.6 of the Official Plan that addresses cultural heritage conservation. Below are portions of Section 4.6 that pertain specifically to built heritage resources and cultural heritage landscapes:

4.6.1 Significant built heritage resources, and significant cultural heritage landscapes, will be conserved.

4.6.2 The County will work with local municipalities and heritage committees to create and maintain an inventory of local and significant cultural features including but not limited to:

- a) Heritage resources designated under Parts IV and V of the Ontario Heritage Act;
- b) Sites or areas having historical, archaeological, cultural, scenic, or architectural merit both on land and underwater;
- c) Cemeteries; and,
- d) Other cultural heritage resources of community interest and significance.

4.6.6 Development and site alteration shall not be permitted on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.

4.6.10 Where feasible and desirable, incentives may be provided by local municipalities to land developers in exchange for preservation of significant cultural features. This can be accomplished by permitting increased densities, density transfers, and other means considered appropriate, in exchange for resource preservation, through the application of the relevant provisions of the Planning Act.

4.6.11 Local municipalities are encouraged to:

- a) Establish policies within their official plans that promote and encourage the designation of heritage properties under the Ontario Heritage Act, and include within these policies the criteria as set out in the Ontario Heritage Act amendment 2006 as Regulation #1 to be used to evaluate the architectural and historic significance;
- b) Create and support a heritage committee within their community to deal with heritage matters considered appropriate;
- c) Zone sites containing significant cultural features sites to ensure preservation in accordance with Section 34(1) 3.3 of the Planning Act; and
- d) Apply the objectives and criteria set out in the County's Cultural Heritage Guidelines.

4.6.12 When burial places are identified during the development process or are encountered during any excavation activity, the provisions of the Funeral, Burial and Cremation Services Act, Ontario Heritage Act and the relevant regulations must be followed. Licensed archaeologists may be involved in heritage burial assessments for delineation of boundaries and excavations if required. Appropriate Provincial Ministries and authorities will be notified.

2. Description of the Property

The bridge is situated on the road allowance between Lot 15, Concession 3 and Lot 15, Concession 4, Tay Township, Simcoe County, Ontario (Image 1). According to available engineering drawings the bridge was constructed c. 1976 (Reid and Associates Ltd. 1976) and is an example of slab on box girder design (Engineered Management Systems Inc. 2016). The bridge spans approximately 40 metres and carries two lanes of traffic. The bridge carries traffic over the Trans Canada Trail that now occupies the former CN rail line that was abandoned c. 1994. The structure has a maximum width of approximately 9 metres and stands approximately 10 metres above the paved mixed-use trail below.

The bridge is located 0.3 kilometres southeast of Highway 12 on Old Fort Road (Simcoe County Road 58). The bridge sits in a slight valley surrounded by a mix of residential and agricultural properties. The design of the bridge permits it to blend into the surrounding countryside; as such the bridge presents a minimal visual impact from Old Fort Road (Image 2, Image 3). When viewed from the Trans Canada Trail the structure presents a more imposing presence but is unremarkable (Image 4).

Due to the low profile of the bridge and its location within a valley it is not visible from any distance and as such does not serve as a local landmark.

The bridge is not currently listed or designated by the municipality.

2.1 Architecture and Design

Old Fort Bridge is an example of Slab On Box Girder design using reinforced concrete and corrosion-resistant cast railing components. The structure is unaltered and displays parapet walls with double tube railing, typical of the construction period. The engineering requirements of the structure were stamped by Professional Engineering H. Borgdorff of Reid and Associates Ltd. Consulting Engineers (Reid and Associates Ltd. 1976). The design of the pre-cast concrete components was undertaken by Pre-Con Company (Pre-Con 1977). The bridge spans approximately 40 metres and carries two lanes of traffic. The bridge carries traffic over the Trans Canada Trail that now occupies the former CN rail line that was abandoned c.1994. The structure has a maximum width of approximately 9 metres and stands approximately 10 metres above the paved mixed-use trail below.

2.2 Current Conditions

The bridge is located 0.3 kilometres southeast of Highway 12 on Old Fort Road (Simcoe County Road 58). The bridge sits in a slight valley surrounded by a mix of residential and agricultural properties. The design of the bridge permits it to blend into the surrounding countryside; as such the bridge presents a minimal visual impact from Old Fort Road (Image 2, Image 3). When viewed from the Trans Canada Trail the structure presents a more imposing presence but is unremarkable (Image 4).

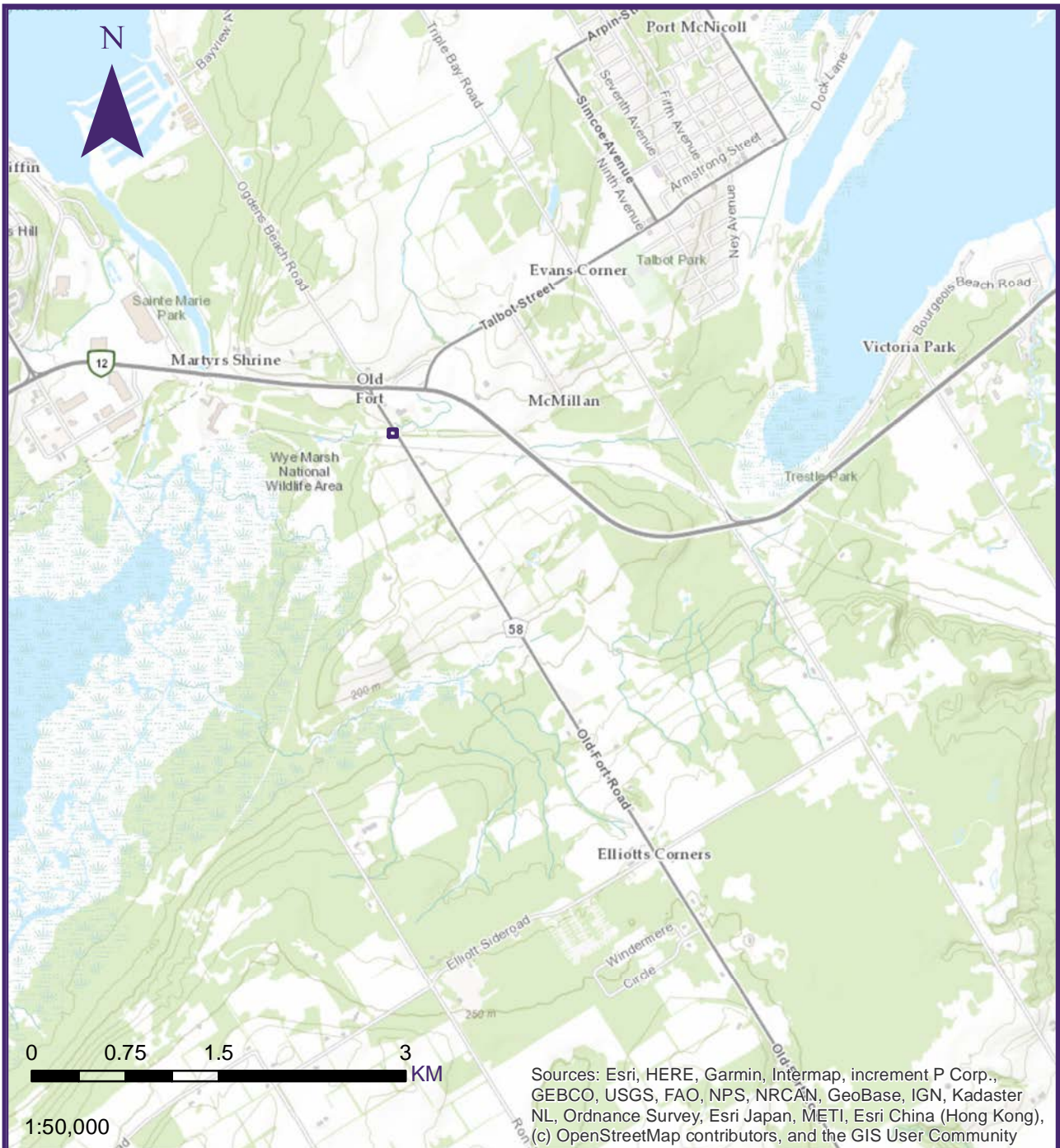
Due to the low profile of the bridge and its location within a valley it is not visible from any distance and as such does not serve as a local landmark.

The structure appears to be in fair condition. There is obvious degradation to the road surface and the cast iron rail displays signs of deformation (Image 5); additionally, several connecting bolts have been dislodged from the cast iron railing support brackets. Portions of the bridge, including the underside, display signs of water infiltration in the form of rust stains and surface spalling of the concrete (Image 7). The grading associated with the rail cut on either side of the bridge support low ground cover and small diameter tree growth.

2.3 Adjacent Lands

The Old Fort Road Bridge is part of the regionally owned collector roadway. No parcels adjacent to the bridge contain any municipal, provincial, or federal heritage status properties.

Map 1: Study Area on Topographic Map



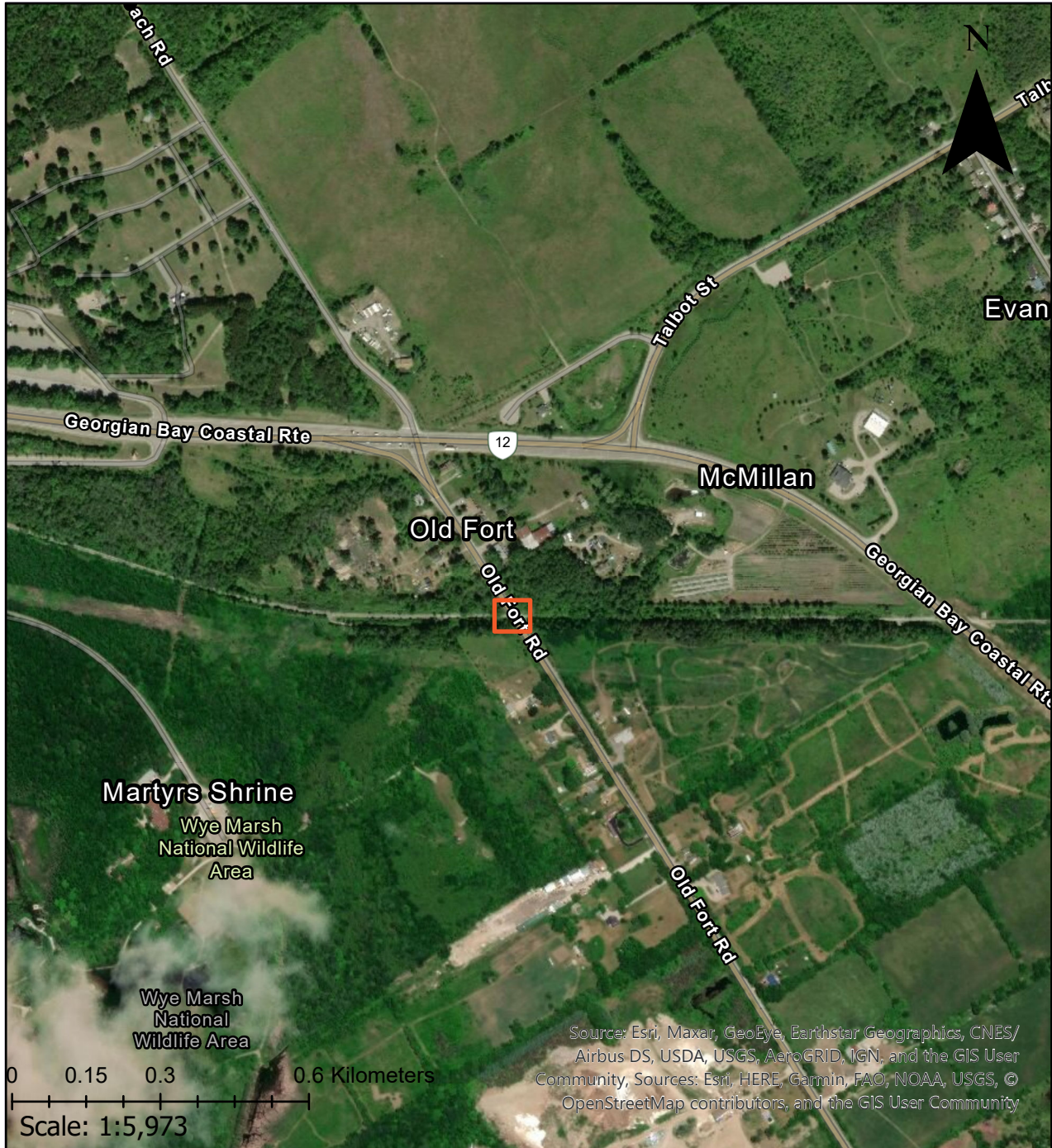
Cultural Heritage Evaluation Report - Old Fort Bridge Replacement, Part Lot 15, Concessions 3 and 4, Tay Township, County of Simcoe

Legend

 Old Fort Bridge



Map 2: Study Area on Aerial



Cultural Heritage Evaluation Report - Old Fort Bridge Replacement, Part Lot 15, Concessions 3 and 4, Tay Township, Simcoe County

Legend

 Old Fort Bridge



3. Historical Research and Analysis

3.1 Settlement History – County of Simcoe

The northern part of Simcoe County in the seventeenth century was the home of the Hurons, an agrarian tribe allied with the French. French priests, first the Jesuits and then the Recollets, established missions near Georgian Bay. The Iroquois from upper New York state under the influence of the British, destroyed the missions and most of the people of Huronia. A few survivors fled to Christian Island in Georgian Bay (Simcoe County Branch, 2020).

During the late seventeenth and the eighteenth century, the homeland of the Hurons became occupied by the Algonquin-speaking peoples, such as the group who eventually became the Chippewas of Rama. French or Metis fur traders made excursions into this territory and bought furs from the Algonquins who had usually obtained the furs from more westerly and northern tribes (Simcoe County Branch, 2020).

In 1815, a group of Scots from Sutherlandshire, who had been brought by Lord Selkirk to the Red River in Manitoba, made the long trek by rivers and lakes to the mouth of the Nottawasaga on Georgian Bay. Then they travelled from there overland and formed the first permanent European settlement in Simcoe County in West Gwillimbury (Simcoe County Branch, 2020).

After the War of 1812, the British became aware of the vulnerability of Upper Canada to American attack. The government began to plan alternate routes in the colony. In 1828, a peace treaty gave Drummond Island to the United States and the population of this outpost were evacuated to Penetanguishene. Some of the soldiers were given small grants of land. Also, after Waterloo, a few persons settled along the Penetanguishene Road in the townships of Tiny, Tay, Medonte, Flos, Oro and Vespra between 1817 and 1820. By 1820, most of the townships were surveyed (Simcoe County Branch, 2020).

The largest influx of settlers came from the British Isles to Simcoe County in 1831 and 1832. In some places former soldiers were given free land grants, some groups such as the Roman Catholic Irish in Adjala or the “Dalhousie settlers” in Innisfil settled on adjacent farms, but, most of these people were individual settlers such as weavers and labourers who had been impoverished by the depression following the Napoleonic Wars. This stream of settlers continued into the 1840’s at a somewhat slower rate (Simcoe County Branch, 2020).

During this period of settlement, today’s Simcoe County was a part of the Home District, the Simcoe District, then Simcoe County. The boundaries of the County were changed from time to time as when Muskoka, and such townships as St. Vincent, were part of Simcoe. In 1975, the townships of Rama and Mara in Ontario County, became part of Simcoe. In 1853, the first Canadian railway was completed, travelling from Toronto to Collingwood via Bradford and Barrie and in the next few decades, the railway was extended to Orillia and Midland. In the 1860’s, new settlers were often Canadian born, coming from the older more established parts of the province (Simcoe County Branch, 2020).

Tay Township

Tay Township was surveyed in 1820 by James G. Chewett, (Tay Township Heritage 2012). It was at this time that the basis for the current road grid was established. Given the rural setting of the bridge it is reasonable to assume that Old Fort Road Bridge is located within the original right-of-way (ROW) established between Lot 15, Concession 3 and Lot 15, Concession 4. In 1879 the rail corridor was established, bisecting the previously established road allowance. The first railway to operate on the corridor was Midland Railway of Canada. In 1893 Midland Railway of Canada was acquired by Grand Trunk Railway. Grand Trunk Railway operated on this line until 1923, when bankruptcy resulted in its incorporation into the Canadian National (CN) Railway (Andreae 1998; TRHA 2020). CN operated the line until its abandonment in 1994. The rail corridor was subsequently redeveloped into Trans Canada Trail.

Engineering documents related to the construction of the bridge are dated July 1976 (Reid and Associates Ltd. 1976); this contrasts with the date of construction listed in the Biennial Inspection Report conducted in 2016. The Biennial Inspection Report lists the date of construction as 1970 (Engineered Management Systems Inc. 2016). It was not possible to confirm which of these dates was correct and the bridge itself was not inscribed with any information.

Review of topographic mapping from 1950 illustrates a bridge in the same location crossing the CN rail line (DND 1950). As the current structure was not constructed until c. 1970, the 1950 topographic map clearly illustrates that the current structure is not the first bridge constructed in this location. Earlier depictions of the study area clearly depict the rail line and Old Fort Road, but do not contain the detail necessary to discern any infrastructure that may have been associated with the crossing.

Part Lot 15 Concession 3 and 4

To understand the history of Euro-Canadian settlement, earlier mapping was consulted. The *1881 Illustrated Historical Atlas of the County of Simcoe, Ontario* illustrates there is Mud Lake to the west of Concession 3, Lot 15 and a stream running from the north to south-west of the lot and another stream running from the north to the east of the lot into the west of Concession 4, Lot 15. There are no landowners, structures shown in the corresponding lot and concessions in this map (Belden, 1881). However, a railroad passes through the north-east corner of Concession 3, Lot 15 and comes through Concession 4, Lot 15 north-west corner and travels down south-east.

Based on the 1871 George Tremaine Map of the Simcoe County, Mud Lake to the west of Concession 3, Lot 15 still resides from 1871 to 1881. In the Tremaine Map over half of Concession 3, Lot 15 is covered by Mud Lake, the east side of the lot is owned by a man named G. Cook. Concession 4, Lot 15 is the same between 1871 and 1881. Though the railroad hasn't been built in the 1871 Map and the streams that appeared in the 1881 Map are not present in the 1871 Map because of Mud Lake which is overlapping the lot and extended further east compared to the 1881 Map.

3.2 Archaeological Potential

Parslow Heritage Consultancy Inc. (PHC) completed a Stage 1 archaeological background assessment and Stage 2 Property survey on behalf of the County of Simcoe, as part of a Schedule B Municipal Class Environmental.

Based on the findings of the Stage 1 archaeological background assessment, it was determined that Stage 2 archaeological survey is recommended for the study area. For the purposes of the Old Fort Road Municipal Class Environmental Assessment, once a conceptual or preliminary design for the selected alternative is completed, Stage 2 archaeological property survey is recommended.

The study area has been extensively disturbed by previous activities, including the construction of the current bridge structure and Old Fort Road itself. Test pit survey was conducted at 5 metre intervals across the study area where there was any probability of archaeological potential. Only a total of four test pits contained undisturbed soils, however no archaeological materials were recovered. It is concluded that the study area does not retain any cultural heritage value or interest and further archaeological work is not recommended.

Figure 3: Location of Old Fort Road Overhead Bridge on 1950 Topographic Map



4. Community Engagement


The following engagement activities were completed as part of the Environmental Assessment:

- ▶ Notice of Study Commencement letters were distributed to all stakeholders, include First Nation communities on March 11, 2020 – *no comments were received regarding archaeological or cultural heritage resources;*
- ▶ Notice of Public Information Centre was distributed to all stakeholders, include First Nation communities on August 18, 2021 – *no comments were received regarding archaeological or cultural heritage resources;*
- ▶ The Public Information Centre included virtual presentation boards on the County website that were available for review from August 23, 2021 to September 11, 2021 (see attached archaeological and cultural heritage board) – *no comments were received regarding archaeological or cultural heritage resources;* and,


A future Notice of Study Completion to be issued closer to project completion. Below is the *slide* created for the Public Information Centre.

EXISTING ENVIRONMENTAL CONDITIONS

- ▶ A Stage 1 and 2 Archaeological Assessment was undertaken. The results of the assessment determined that the study area does not retain any archaeological heritage value or interest.
- ▶ A Cultural Heritage Evaluation Report was prepared. The results of this assessment concluded that bridge does not retain any cultural heritage value or interest.



Test pits as part of the Stage 2 archaeological assessment.



View of bridge from the trail.



4

5. Assessment of Existing Conditions

5.1 Criteria for Determining Cultural Heritage Value or Interest

Ontario Regulation 9/06 prescribes the criteria for determining the Cultural Heritage Value or Interest (CHVI) of a property. The regulation requires that, to be designated, a property must meet “one or more” of the criteria grouped into the categories of Design/Physical Value, Historical/Associative Value and Contextual Value (MHSTCI 2006a). Table 1 lists these criteria and identifies if the criteria were met for Simcoe County Structure # 058086; these criteria categories are expanded on below.

Table 1: The criteria for determining property of Cultural Heritage Value or Interest (CHVI)

O.Reg.9/06 Criteria	Criteria Met (Y/N)	Justification
The property has design value or physical value because it,		
I. is a rare, unique, representative or early example of a style, type, expression, material, or construction method,	N	The bridge is of common construction and does not represent an early example of style, type, material use or construction method.
II. displays a high degree of craftsmanship or artistic merit, or	N	The bridge does not display a high degree of craftsmanship or artistic merit.
III. demonstrates a high degree of technical or scientific achievement.	N	The bridge is an example of common concrete construction utilizing both pre-cast and cast-in-place components.
The property has historical value or associative value because it,		
I. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,	N	The bridge has no direct association to any theme, event, belief, person, activity, organization or institution that is significant to the community. Not the original bridge to cross the rail ROW.
II. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or	N	The bridge does not present the potential to yield information that contributes to an understanding of a community or culture.
III. Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.	N	The bridge reflects a common and long-held method of bridge construction and does not reflect the work or ideas of an architect, artist, builder, designer, or theorist who is significant to a community.

The property has contextual value because it,		
I. is important in defining, maintaining or supporting the character of an area,	N	The bridge is located on a secondary road and does not serve as a local landmark.
II. is physically, functionally, visually or historically linked to its surroundings, or	N	The bridge does not present a physical, functional, visual or historical link to its surroundings.
III. is a landmark.	N	The bridge is barely visible from the surrounding landscape. Review of historic references does not suggest it is a landmark.

5.1.1 Design Value or Physical Value

As indicated in Table 1, the bridge is of common construction and does not represent an early example of style, type, material use or construction method. Furthermore, the bridge is an example of common concrete construction utilizing both pre-cast and cast-in-place components.

Based on this information, there is no design or physical value attributed to Simcoe County Structure # 058086.

5.1.2 Historic Value or Associative Value

Simcoe County Structure # 058086 is not the original overhead structure. The bridge does not have any direct association to any theme, event, belief, person, activity, organization or institution that is significant to the community and does not have the potential to yield information that contributes to an understanding of a community or culture.

5.1.3 Contextual Value

Simcoe County Structure # 058086 is not a landmark on the landscape, and while it is an overhead bridge for a historic railway, the railway is no longer used for this purpose and the original bridge associated with the railway was replaced with the current structure.

5.1.4 Archaeological Value

The Subject Property has been assessed for archaeological value. The archaeological assessment did not recommend any further work (PHC 2020).

5.1.5 Historic Photographs

Attempts were made to locate images of the bridge from the time of construction in 1976 but none were identified.

6. Conclusions

Simcoe County Structure # 058086 was evaluated against Ontario Regulation 9/06 prescribes the criteria for determining the Cultural Heritage Value or Interest (CHVI) of a property and was found to have no cultural heritage value or interest. As a result of the CHER for Old Fort Road Overhead Bridge (Simcoe Structure #058086), the following recommendation is made:

1. The Old Fort Road Overhead Bridge (Simcoe Structure #058086) does not exhibit cultural heritage value or interest and as such no further cultural heritage assessments are recommended for this structure.

7. Photos



Image 1: Overview of Old Fort Road Overhead Bridge, facing southeast



Image 2: West approach to bridge, facing east



Image 3: East approach to bridge, facing west



Image 4: Bridge as viewed from Tans Canada Trail, facing south



Image 5: Bridge parapet and guard rail note, deformation of railing, facing south



Image 6: Surface of bridge deck, facing west



Image 7: Example of spalled concrete and water stains on bridge parapet wall

8. Bibliography and Reference Documents

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Pre-Con Company

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Accessed 23 June 2020.

Appendix A



Old Fort Overhead Bridge General Arrangement

DRAWING NAME: F:\202328\5. Bridge\Drafting\202328_Old Fort-64-S01-Option 1.dwg
 CREATED: 2020-12-29 12:29:30pm

MINISTRY OF TRANSPORTATION, ONTARIO
 PP-6-707

BB-05

METRIC
 DIMENSIONS ARE IN METRES
 AND/OR MILLIMETRES
 UNLESS OTHERWISE SHOWN

CONT No
 WP No 4078-14-01



OLD FORT OVERHEAD BRIDGE
 GENERAL ARRANGEMENT
 OPTION 1: SINGLE SPAN BRIDGE

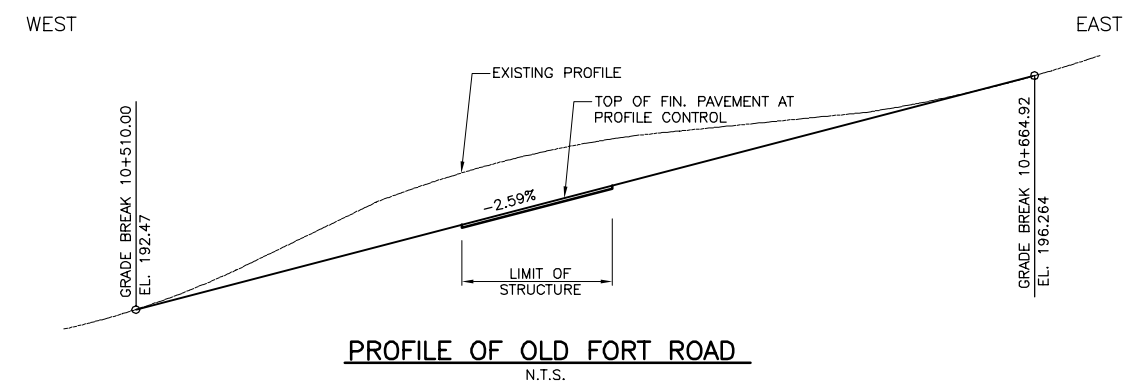
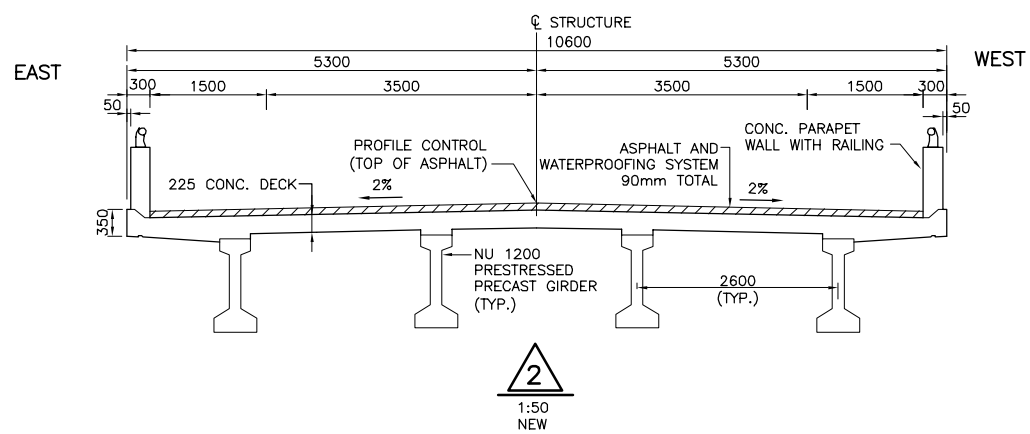
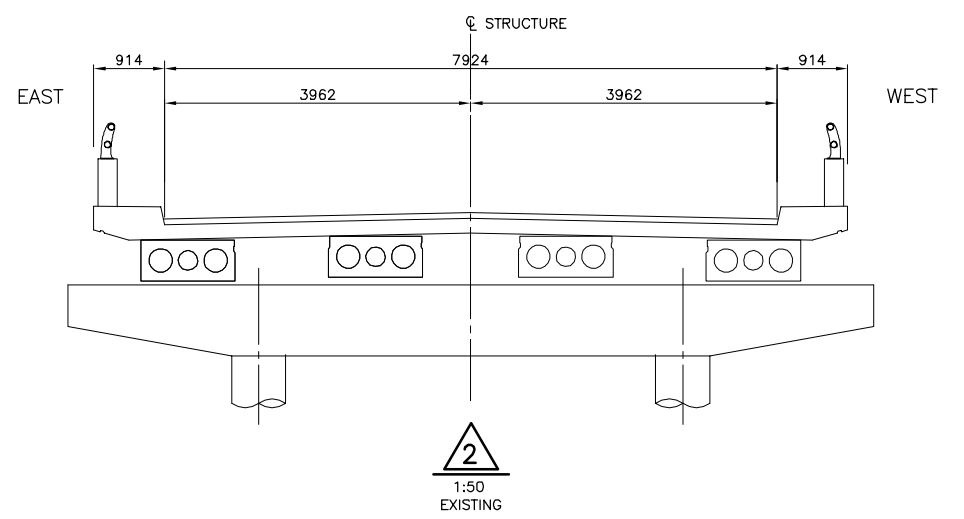
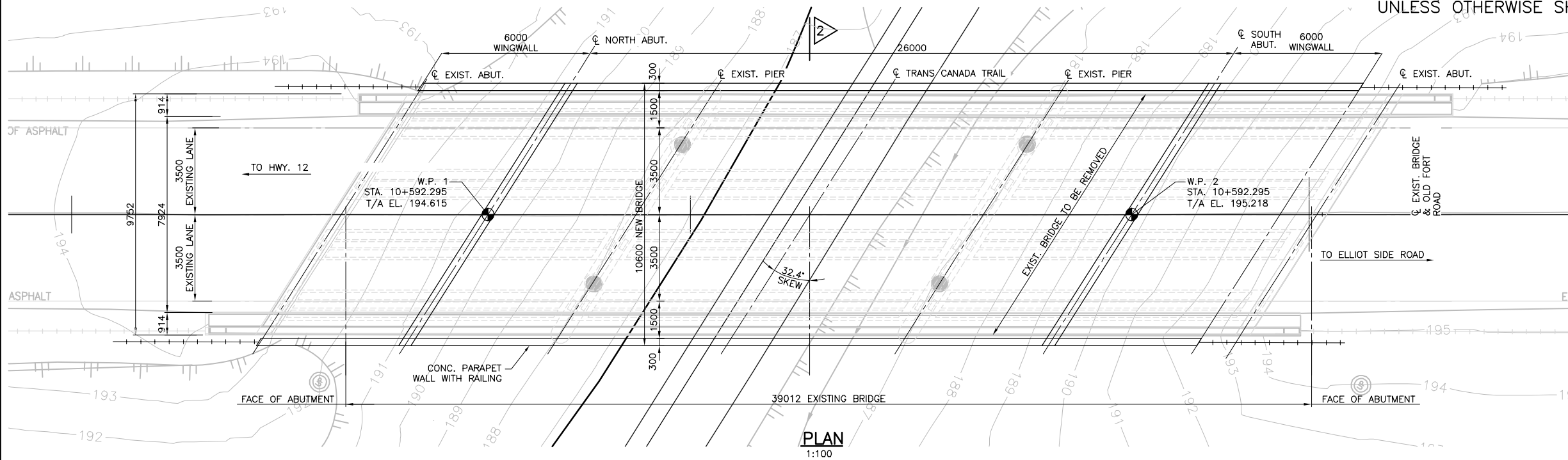
SHEET
 -

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GENERAL NOTES:

- CLASS OF CONCRETE**
 GIRDERS.....65 MPa
 REMAINDER.....30 MPa
- CLEAR COVER TO REINFORCING STEEL**
 FOOTING.....100±25
 DECK TOP.....70±20
 BOTTOM.....40±10
 REMAINDER (UNLESS NOTED OTHERWISE).....70±20
- REINFORCING STEEL**
 REINFORCING STEEL SHALL BE GRADE 400W UNLESS OTHERWISE SPECIFIED.
 TENSION LAP LENGTHS NOT INDICATED ON THE CONTRACT DRAWINGS SHALL BE CLASS B.
 BAR MARKS WITH PREFIX "S" DENOTE STAINLESS STEEL BARS.
 BAR HOOKS SHALL HAVE STANDARD HOOK DIMENSIONS USING MINIMUM BEND DIAMETERS, WHILE STIRRUPS AND TIES SHALL HAVE MINIMUM HOOK DIMENSIONS. ALL HOOKS SHALL BE IN ACCORDANCE WITH THE STRUCTURAL STANDARD DRAWING SS12-1, UNLESS INDICATED OTHERWISE.
- CONSTRUCTION**
 - THE CONTRACTOR SHALL ESTABLISH THE BEARING SEAT ELEVATIONS BY DEDUCTING THE ACTUAL BEARING THICKNESSES FROM THE TOP OF BEARING ELEVATIONS. IF THE ACTUAL BEARING THICKNESSES ARE DIFFERENT FROM THOSE GIVEN WITH THE BEARING DESIGN DATA, THE CONTRACTOR SHALL ADJUST THE REINFORCING STEEL TO SUIT.
 - BACKFILL BEHIND ABUTMENTS SHALL NOT BE PLACED UNTIL CONC. IN THE DECK HAS REACHED A STRENGTH OF 25 MPa.
 - BACKFILL SHALL BE PLACED SIMULTANEOUSLY BEHIND BOTH CONCRETE ABUTMENTS KEEPING THE HEIGHT OF THE BACKFILL APPROXIMATELY THE SAME. AT NO TIME SHALL THE DIFFERENCE IN ELEVATION BE GREATER THAN 500mm.
 - THE CONTRACTOR SHALL ENSURE THE STABILITY OF THE ABUTMENTS AND GIRDERS UNTIL THE CONC. DECK HAS REACHED A STRENGTH OF 25 MPa.



WP #	STATION	NORTH COORDINATE	EAST COORDINATE
1	10+592.295	4953961.072	592507.091
2	10+566.295	4953939.234	592521.202

DRAWING NOT TO BE SCALED
 100 mm ON ORIGINAL DRAWING

REVISIONS		DATE		BY		DESCRIPTION	

DESIGN	MR	CHK	PC	CODE	CHBDC 19	LOAD ONT CL-625	DATE	JUNE 2020
DRAWN	DT	CHK	MR	SITE	STRUCT	SCHEME	DWG	1

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